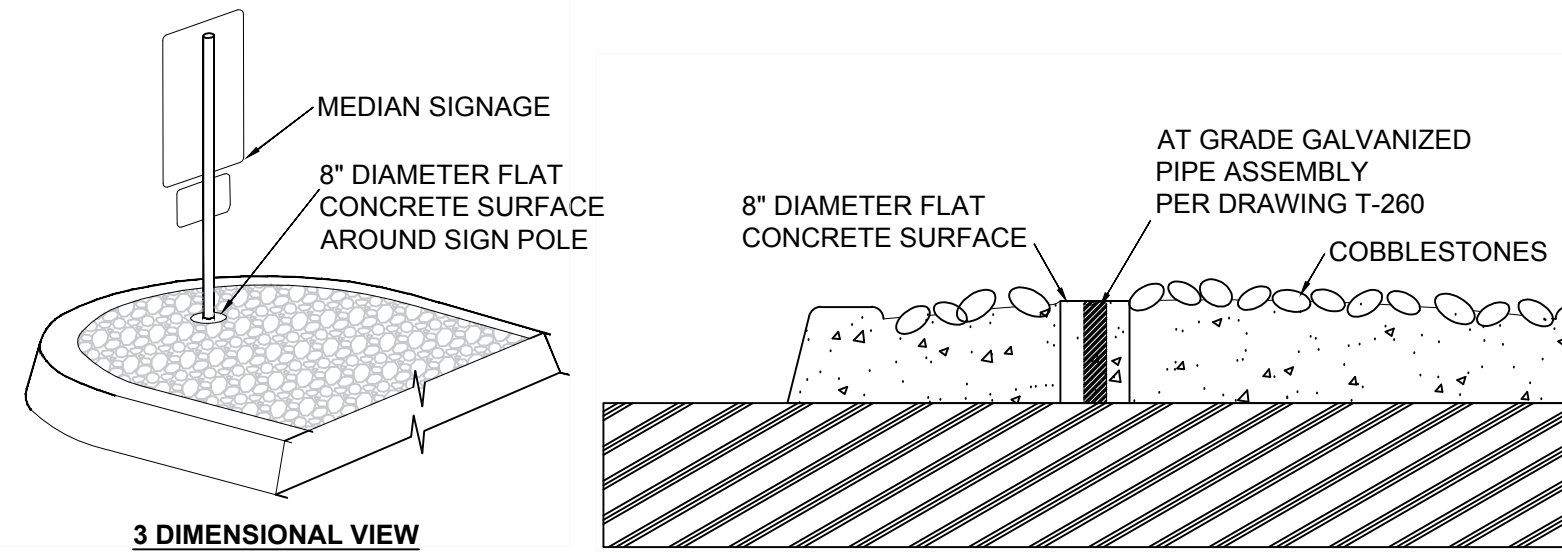


SIGNING AND STRIPING GENERAL NOTES:

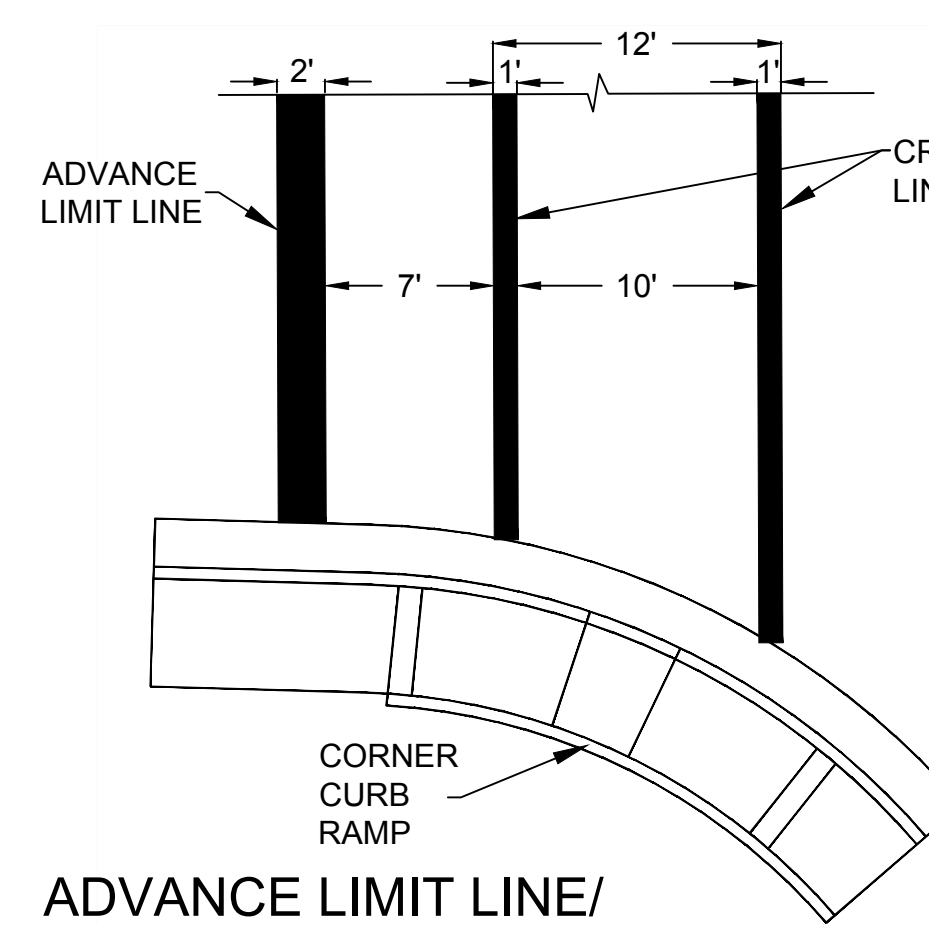
- ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CITY OF SACRAMENTO DESIGN AND PROCEDURES MANUAL, DATED SEPTEMBER 1990, THE CITY OF SACRAMENTO STANDARD SPECIFICATIONS, DATED JUNE 2007, CALTRANS STANDARD PLANS, 2015 EDITION, AND THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CA MUTCD), 2014 EDITION.
- THIS PLAN IS ACCURATE FOR SIGNING AND STRIPING WORK WITHIN THE PUBLIC RIGHT OF WAY ONLY.
- ALL EXISTING SIGNING, STRIPING, AND MARKINGS TO REMAIN, UNLESS OTHERWISE NOTED. CONFLICTS BETWEEN EXISTING AND PROPOSED SHALL BE RESOLVED BY THE ENGINEER.
- ALL STRIPING AND MARKINGS SHALL BE THERMOPLASTIC, UNLESS NOTED OTHERWISE. 3M TAPE (OR APPROVED EQUIVALENT) SHALL BE USED ON CONCRETE SURFACES.
- EXACT POSITION AND LOCATION OF ALL ROADSIDE SIGNS SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- REMOVAL OF EXISTING STRIPING AND PAVEMENT MARKINGS SHALL BE ACCOMPLISHED BY APPROVED GRINDING METHOD.
- CROSSWALKS SHALL BE 12 FEET WIDE INCLUDING THE 12" SOLID PAVEMENT MARKINGS AND SHALL BE WHITE UNLESS OTHERWISE NOTED.
- ALL SIGNING AND STRIPING WORK SHALL BE COORDINATED WITH THE ELECTRICAL WORK AS DIRECTED BY THE ENGINEER.
- LANE WIDTHS ADJACENT TO CURBS ARE MEASURED TO THE FACE OF CURB.
- TRAFFIC SIGNS SHALL BE INSTALLED BEHIND THE SIDEWALK WHEN THE SIDEWALK IS IMMEDIATELY ADJACENT TO THE CURB. SIGNS SHALL BE INSTALLED BETWEEN THE CURB AND SIDEWALK WHEN THE SIDEWALK IS DETACHED. SIGNS IN PEDESTRIAN AREAS SHALL HAVE A CLEARANCE FROM THE GROUND OF AT LEAST SEVEN FEET. IF THIS CLEARANCE REQUIREMENT IS NOT SATISFIED AFTER THE INSTALLATION OF ADDITIONAL SIGN PANELS, CONTRACTOR SHALL INSTALL A NEW SIGN POST.
- ALL ROADWAY SIGNS (REGULATORY, WARNING, GUIDE, SCHOOL ZONE AND OBJECT MARKERS) SHALL INCLUDE ASTM TYPE XI SHEETING. FOR ALL OTHER SIGNS, USE ASTM TYPE II SHEETING. FLUORESCENT YELLOW-GREEN SHALL NOT BE USED IN ANY SIGNS OTHER THAN SCHOOL RELATED SIGNAGE.
- MOUNT SIGNS USING BANDING ON SIGNAL AND STREET LIGHT POLES WHERE FEASIBLE. SEE ELECTRICAL PLANS FOR POLE LOCATIONS.
- CONTRACTOR SHALL VERIFY WITH THE ENGINEER THE EXACT STREET NAME AND STREET ADDRESS FOR PLACEMENT ON STREET NAME SIGNS PRIOR TO ORDERING SIGNS.
- ALL SIGNS REGULATING PARKING SHALL BE DOUBLE SIDED AND SIDE MOUNTED, UNLESS SHOWN OTHERWISE.
- AT NEWLY SIGNALIZED LOCATIONS, REMOVE EXISTING STOP SIGNS, STOP LEGENDS AND STANCHIONS AT THE TIME SIGNAL IS TURNED ON.
- CONTRACTOR SHALL DELIVER ALL REMOVED ROADWAY SIGNS TO THE CITY OF SACRAMENTO SOUTH AREA CORPORATION YARD AT 5730 24TH STREET, BLDG 10, 95822, PHONE (916) 808-4038

LEGEND:

- ★ INSTALL ROADSIDE SIGN
- ◆ REMOVE ROADSIDE SIGN
- ▲ RELOCATE EXISTING SIGN
- EXISTING ROADSIDE SIGN TO REMAIN
- ↓ EXISTING ROADSIDE SIGN LOCATION
- ↓ PROPOSED ROADSIDE SIGN LOCATION (CENTER MOUNTED ON POLE)
- PROPOSED ROADSIDE SIGN LOCATION (SIDE MOUNTED ON POLE)
- MAS MAST ARM MOUNTED SIGN
- ### REMOVE EXISTING TRAFFIC STRIPING OR PAVEMENT MARKING
- EXISTING SIGNAL/STREET LIGHT POLE
- PROPOSED SIGNAL/STREET LIGHT POLE
- PROPOSED SIGNAL POLE WITH MAST ARM
- ## CA MUTCD STRIPING DETAIL NUMBER WITH LENGTH IN FEET
- ## EXISTING STRIPING WITH CA MUTCD STRIPING DETAIL NUMBER
- BIKE LANE PAVEMENT MARKING PER CA MUTCD FIGURE 9C-3(C)
- TYPE IV, PAVEMENT ARROW (LEFT OR RIGHT)
- PCC PAVEMENT



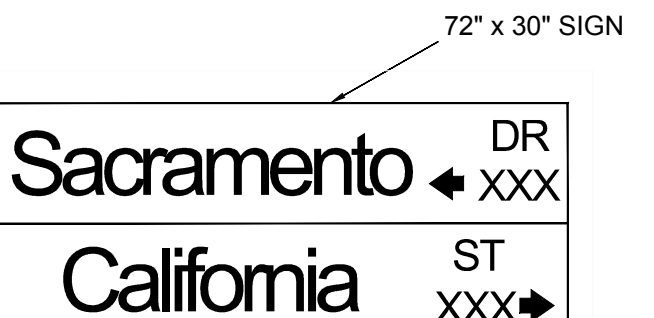
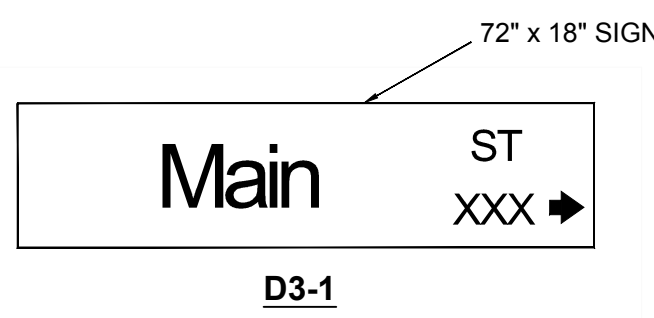
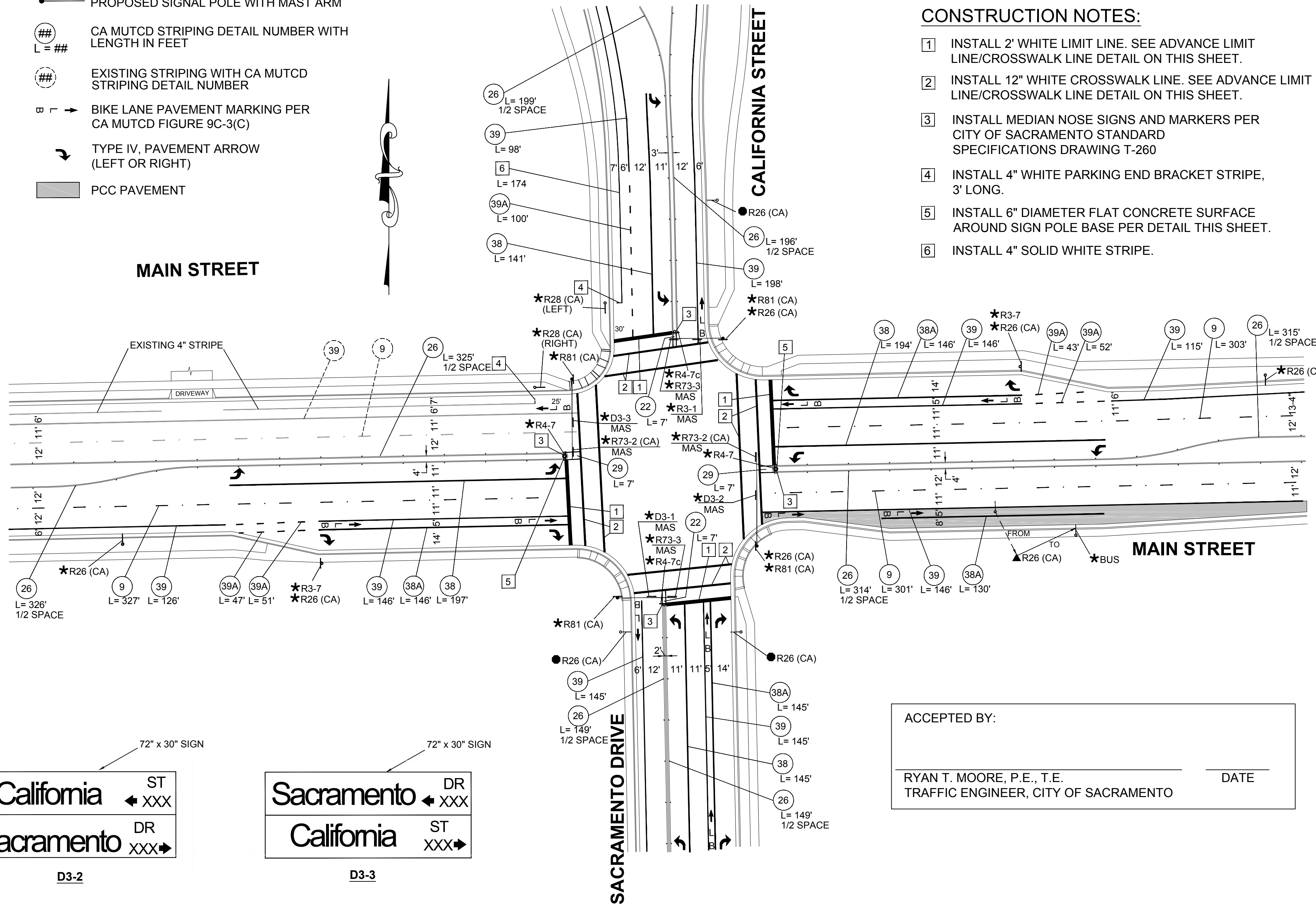
MEDIAN SIGNAGE INSTALLATION
NOT TO SCALE



ADVANCE LIMIT LINE/ CROSSWALK LINE DETAIL
NOT TO SCALE

CONSTRUCTION NOTES:

- INSTALL 2' WHITE LIMIT LINE. SEE ADVANCE LIMIT LINE/CROSSWALK LINE DETAIL ON THIS SHEET.
- INSTALL 12" WHITE CROSSWALK LINE. SEE ADVANCE LIMIT LINE/CROSSWALK LINE DETAIL ON THIS SHEET.
- INSTALL MEDIAN NOSE SIGNS AND MARKERS PER CITY OF SACRAMENTO STANDARD SPECIFICATIONS DRAWING T-260
- INSTALL 4" WHITE PARKING END BRACKET STRIPE, 3' LONG.
- INSTALL 6" DIAMETER FLAT CONCRETE SURFACE AROUND SIGN POLE BASE PER DETAIL THIS SHEET.
- INSTALL 4" SOLID WHITE STRIPE.



ACCEPTED BY:
RYAN T. MOORE, P.E., T.E.
TRAFFIC ENGINEER, CITY OF SACRAMENTO

DATE _____

Helpful Hints for Signing and Striping Plan Preparation
(not to be included on the plans)

- Show existing signage and striping in a gray tone.
- Show proposed signage striping in a bold tone.
- Identify all existing improvements (travel lanes, driveways, medians, bus stops, etc.) on both sides of the street for at least 150 feet beyond the site/project, or as directed by city staff.
- Use existing or proposed electroliers for sign installation when possible.
- Show dimensions in feet and inches only, no decimals and no fractions.
- Travel lane offsets are not preferred. However, if an offset must occur, verify that there is a minimal or acceptable offset based on the design speed.
- Verify that the appropriate design vehicle can make all turning movements.
- Verify that U-turns have 44' available for turning movement. This dimension is measured from the center of the 8 inch stripe at the right side of the left turn lane to the face of the opposing curb.
- Show existing or proposed signs that are on the electrolier or mast arm of the traffic signal.
- Use appropriate pavement arrows and lane line striping based on design speed.
- Median islands should have CA MUTCD Detail 26 placed around it, at half spacing as well as appropriate signage and markers at the "median nose".
- Street elbows shall have CA MUTCD Detail 23 placed along the curved portion of the pavement crown.
- Verify that the marked crosswalks at uncontrolled locations are consistent with the City's Pedestrian Crossing Guidelines (October 2014 version).
- Apply appropriate striping and signage at school zones.
- Avoid using AUTOCAD layers that are not pertinent to the striping plan. For example, turn off underground utilities, survey points, etc.
- Verify that bike lanes are 6 feet wide along any roadway segment and 5 feet wide at intersection through lanes. Bike lanes shall have a minimum of 3 feet of pavement travelway without conflicts between the gutter lip/inlet and the stripe.
- Identify PCC pavement areas that will be striped.
- Typical plans should be drawn at 1" = 40' scale.
- At driveways and alleys, where on-street parking is allowed and stalls are not defined, 4" white parking stripe shall be discontinued for 6 feet before beginning of driveway opening to 6 feet beyond end of driveway opening, and bike lane Detail 39 shall be continuous across driveway opening.
- Street name shall be centered within sign; Street, Drive, Avenue or Court shall be centered over block numbers.

S:\Transportation\Techs\ED\Sample striping plan\Example drawing-public.dwg

IMPROVEMENT PLANS FOR MAIN STREET AND SACRAMENTO DRIVE AND CALIFORNIA DRIVE

NO.	DESCRIPTION	DATE	BY

BENCH MARK	ELEV.

FIELD BOOK	SCALE

CITY OF SACRAMENTO
DEPARTMENT OF PUBLIC WORKS

DRAWN BY: _____	DESIGN BY: _____	CHECKED BY: _____
DATE: _____	R.C.E.: _____ DATE: _____	R.C.E.: _____ DATE: _____



SAMPLE IMPROVEMENT PLANS FOR
MAIN STREET, SACRAMENTO DRIVE AND CALIFORNIA STREET
SIGNING AND STRIPING PLAN
(Date: 7/13/2018)

SHEET	x
OF	x